



# El Camino Real Specific Plan

CAC Meeting – September 17, 2020

# agenda

5:30 – 6:00PM **Presentation**

1. Overview and community input to date
2. Key elements of the Specific Plan
3. Next steps

6:00 – 6:45PM **Q&A Session**

6:45 – 7:00PM **Public comment**

7:00 **Adjourn**

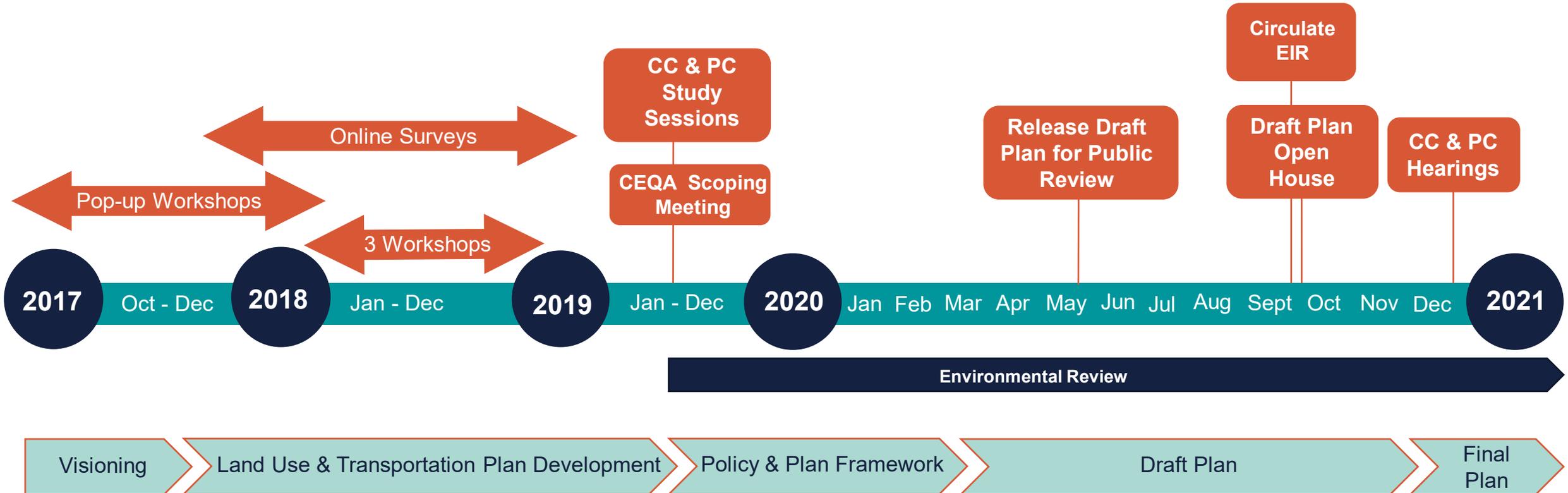
# Project Overview



# What is the project?

- **A Specific Plan** for El Camino Real in Santa Clara
  - Refine and implement the 2035 General Plan vision for the area
  - Create goals, policies, and design standards
  - Identify specific improvements and actions to achieve the vision
  - Address required specific plan topics such as land use, transportation, and infrastructure
- **An Environmental Impact Report (EIR)** to assess potential environmental impacts

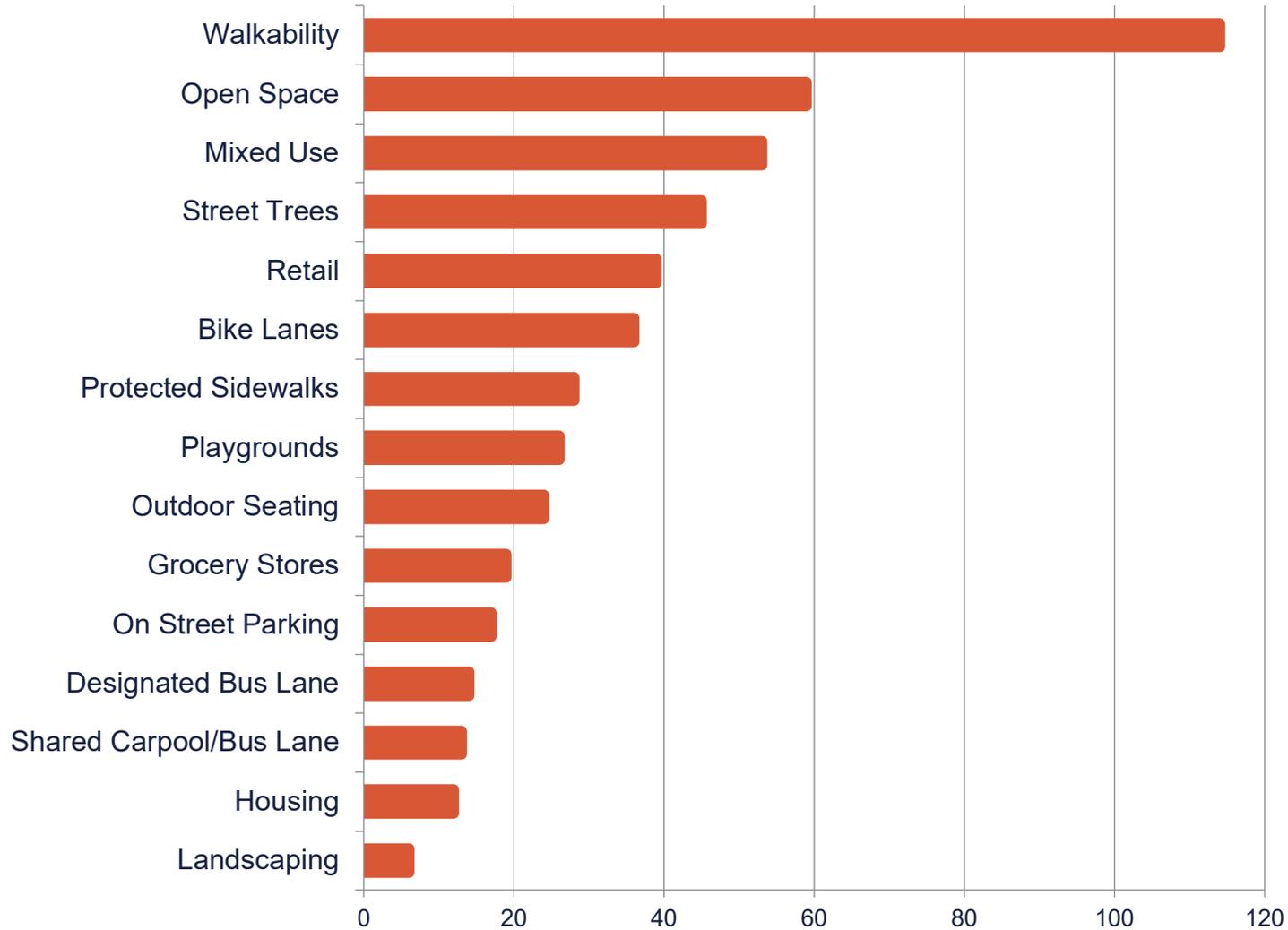
# Project Timeline



# Engagement & Community Input

# Pop Up Event Results

What would you like to see along ECR?

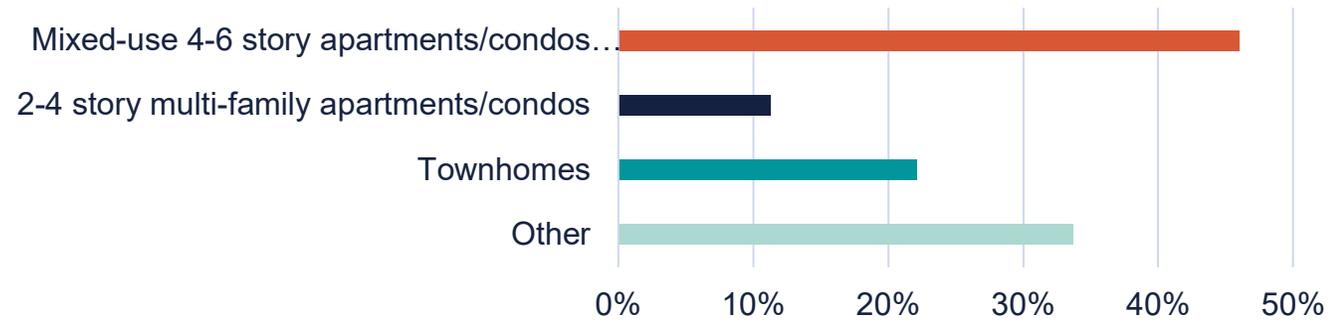


# Online Survey Results

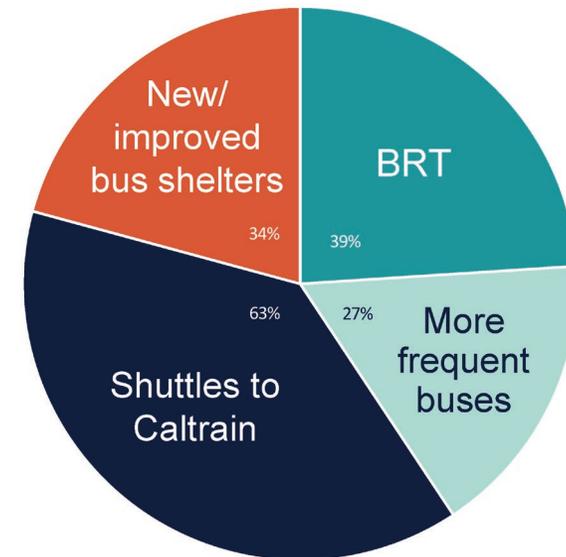
## Top rated issues along ECR

- 1 Traffic congestion
- 2 Lack of street trees
- 3 Lack of parks, community centers, & open space

## What type of housing is most appropriate along ECR?

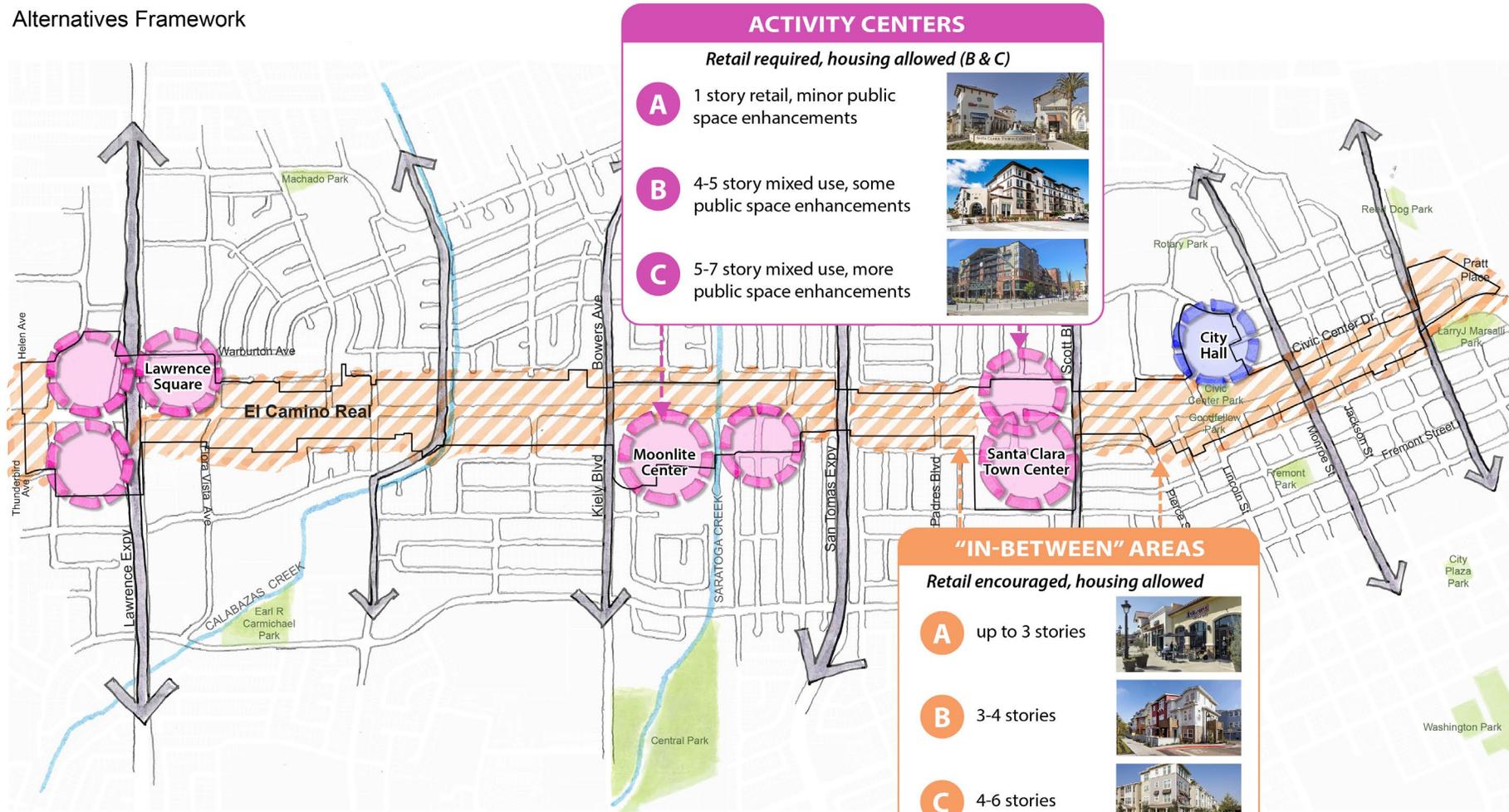


## Top transit improvements



# Workshop 2 - Alternatives

## Alternatives Framework



### ACTIVITY CENTERS

Retail required, housing allowed (B & C)

- A** 1 story retail, minor public space enhancements
- B** 4-5 story mixed use, some public space enhancements
- C** 5-7 story mixed use, more public space enhancements



### "IN-BETWEEN" AREAS

Retail encouraged, housing allowed

- A** up to 3 stories
- B** 3-4 stories
- C** 4-6 stories



### TRANSPORTATION ALTERNATIVES

DIRECTIONS: Place ONE sticky dot next to your preferred street alternative for ECR.

**1** Minor modifications: widen median, narrow traffic lanes, add landscaping, remove pork chop islands at intersections

Place sticky dot here  
3 lanes on ECR side, 2 lanes on San Tomas traffic

Keep 3 lanes of traffic + 1  
ECR has the worst bike/ped collision rates in our entire city. Strongly recommend: - protected bike lane - remove bike/ped safety - remove underutilized on-street parking for improved usability

**2A** Removes on-street parking to accommodate a protected bike lane with bus boarding island

Place sticky dot here  
Bike lanes should not have bus stop signs on merge into the left turn lane and ahead of the light of the red to temporarily close + 1  
- I disagree, bus stop signs safety for all, in bus - remains - remove potential for ECR in the future - I agree + 1

**2B** Remove on-street parking to accommodate a protected bike lane with bus pull-out

Bus lane should be for all public/private buses shuttles + car pool vehicles

Place sticky dot here  
I suggest (use out 2 bike lanes) + 1

**3A** Roadway reallocation A: remove travel lane(s), widen center median, widen sidewalks, add bulb outs

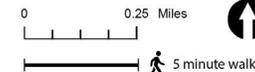
Place sticky dot here  
Wider design not permit right turn bike collisions  
Let bus lanes be bus only + 1

**3B** Roadway reallocation B: remove travel lane(s), keep on-street parking, widen sidewalks, add bulb outs

Place sticky dot here  
AM 2/10/2020  
Wish - 1/10/2020  
13 - 1/10/2020  
@ 10:12 AM

### Legend

- Civic Activity Center
- Activity Center
- "In-Between" Areas
- Major Connections
- Parcels
- Creeks
- Parks
- Project Boundary



## Workshop 1 - 3

# Key Takeaways

## Land Use & Character

- Encourage higher-intensity development at activity centers and lower intensity development in the “in-between” areas along the corridor
- Provide variety of housing choices for a mix of income levels
- Minimize traffic, parking, and privacy impacts on surrounding neighborhoods
- Create destinations
- Support existing retail uses along ECR and encourage higher quality retail and services
- Add parks and public spaces at activity centers

# Transportation & Streetscape

- Improve bicycle and pedestrian safety through signal and crossing treatments
- Improve traffic flow and efficiency
- Improve access and connectivity to Santa Clara Transit Center and to/between surrounding neighborhoods
- Remove on street parking along ECR to accommodate protected bike lanes and bus boarding islands
- Widen sidewalks and add street trees
- Add pedestrian-scale lighting and street furnishings

**Workshop 1 - 3**  
**Key**  
**Takeaways**

# Key Elements of the Plan





# Desired Outcomes

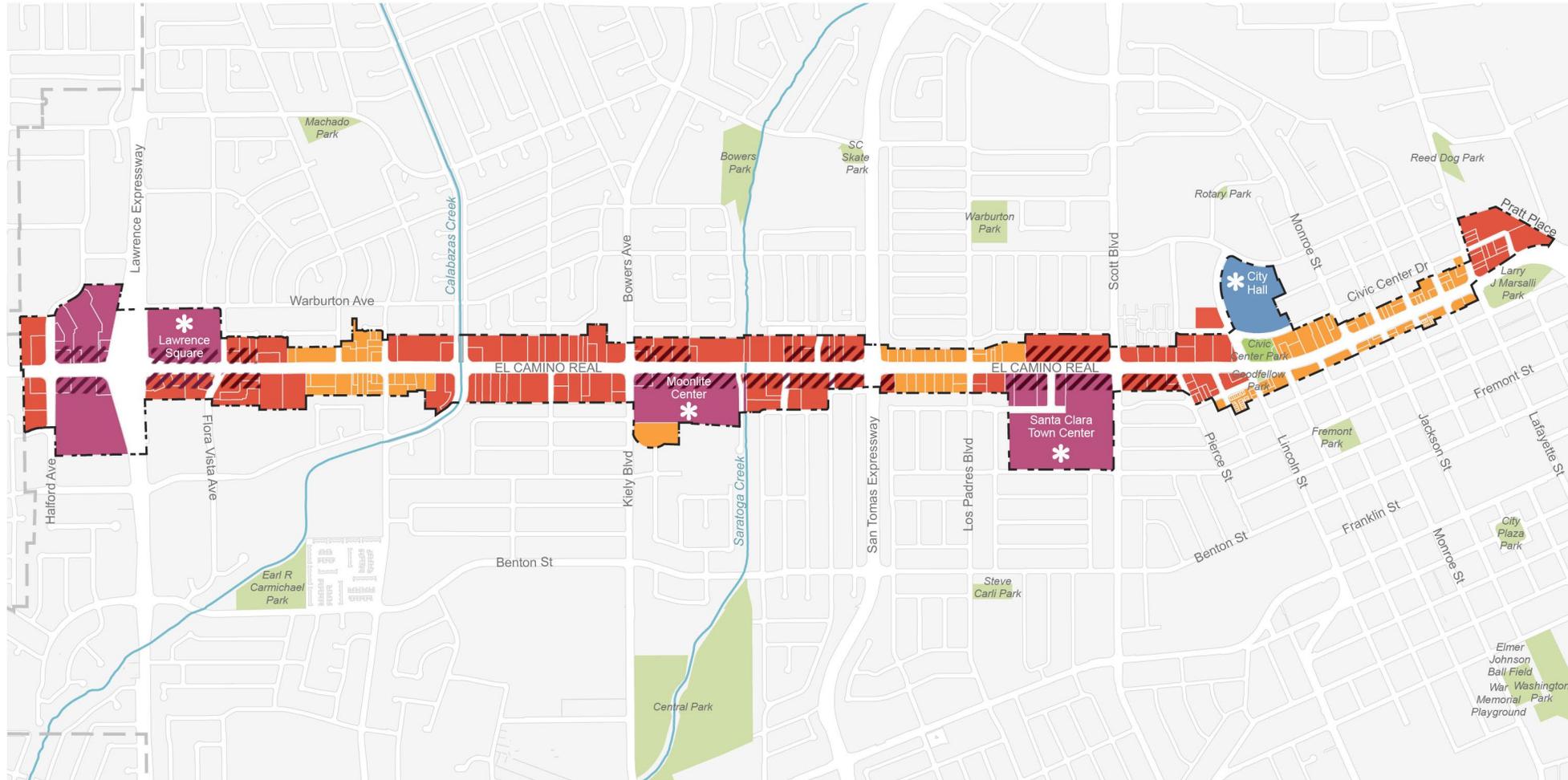
- **More Parks, Plazas, & Open Space**
- **Landscaping & Street Trees**
- **More Walkable Environment**
- **Better Mobility & Connections**
- **More Transportation Options**
- **Efficient & Shared Parking**
- **Compatibility with Adjacent Neighborhoods**
- **Local & Regional Destination**
- **Diversity of Uses**
- **Balanced Approach to Housing**
- **Beautification**
- **Green Building & Sustainable Infrastructure**
- **Support Health & Wellbeing**



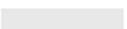
# land use & character areas

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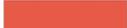
# Land Use Plan



## Legend

-  City Boundary
-  Plan Boundary
-  Parcels
-  Landmarks
-  Parks
-  Creeks

## Land Use Designation

-  Regional Commercial Mixed Use
-  Corridor Mixed Use
-  Corridor Residential
-  Public/Quasi-Public

## Commercial Overlay

-  Ground Floor Commercial Required

0 0.125 0.25 0.5 Miles

 5 minute walk



# Regional Commercial Mixed Use

**Residential Density:** 55-100 dwelling units/acre

**Min Commercial FAR:** 0.2

**Allowed Height:** 5-6 stories

- Retail (1-story or ground floor)
- Ground floor commercial required along ECR
- Pedestrian-oriented frontages and facades
- 5-6-story multifamily housing
- Mostly structured parking
- Large publicly-accessible open space
- Significant public streetscape and pedestrian improvements





# Activity Center Conceptual Diagrams

## Moonlite Center

# Corridor Mixed Use

**Residential Density:** 45-65 dwelling units/acre

**Allowed Height:** 4-5 stories

- Retail (1-story or ground floor)
- Commercial required in specific locations along ECR
- Medium-to-high density residential at smaller cross-streets
- Parking behind buildings, below-grade, or in structures
- Some shared open space



# Corridor Residential

**Residential Density:** 16-45 dwelling units/acre

**Allowed Height:** 3-4 stories

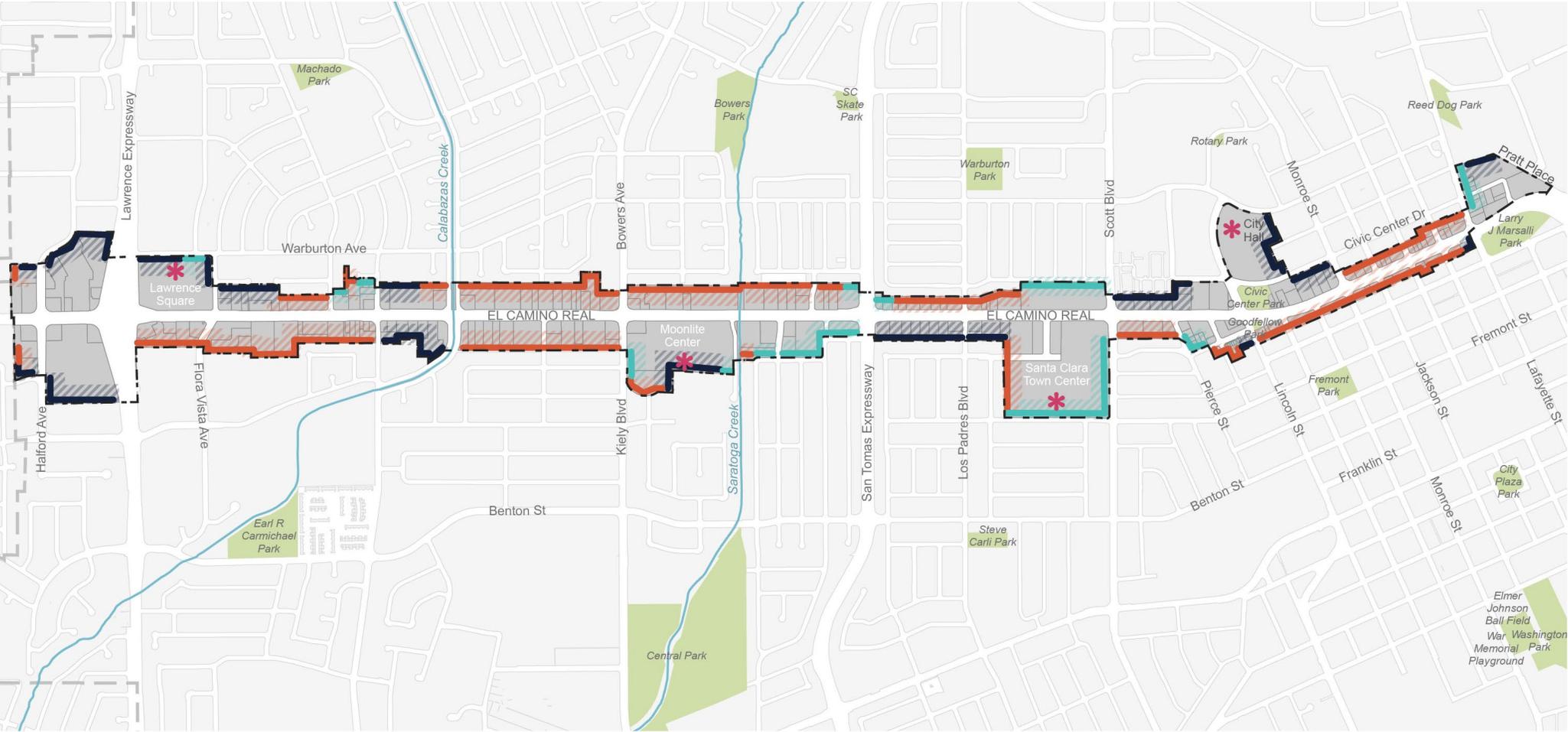
- Low- to mid-rise residential building types (e.g. garden apartments, townhouses)
- Commercial ground floor uses allowed and encouraged, but not required
- Transition to single-family neighborhoods
- Applies to smaller parcels



# neighborhood transitions

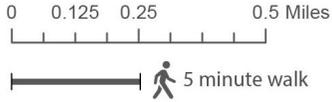
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# Neighborhood Transitions



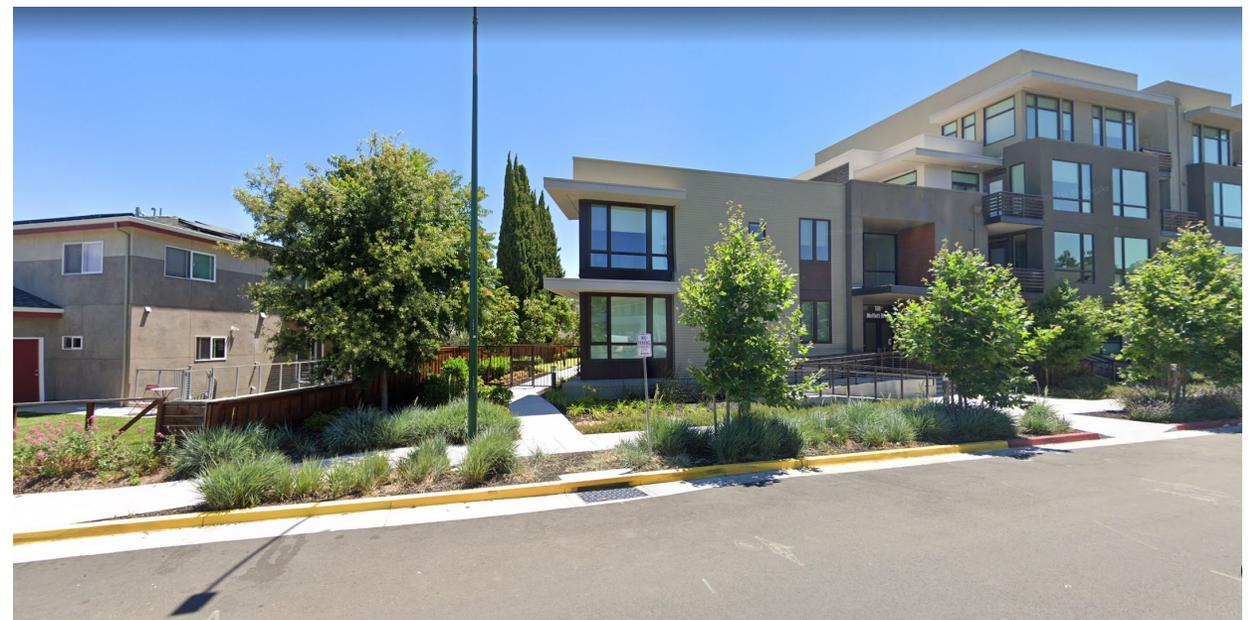
**Legend**

- City Boundary
- - - Plan Boundary
- ▒ Parcels
- ▒ Plan Parcels
- ▒ Parks
- ▒ Creeks
- ▨ Directly abuts a single-family residential parcel
- ▨ Across the street from a single-family residential parcel
- ▨ Directly abuts or across the street from a multi-family residential parcel

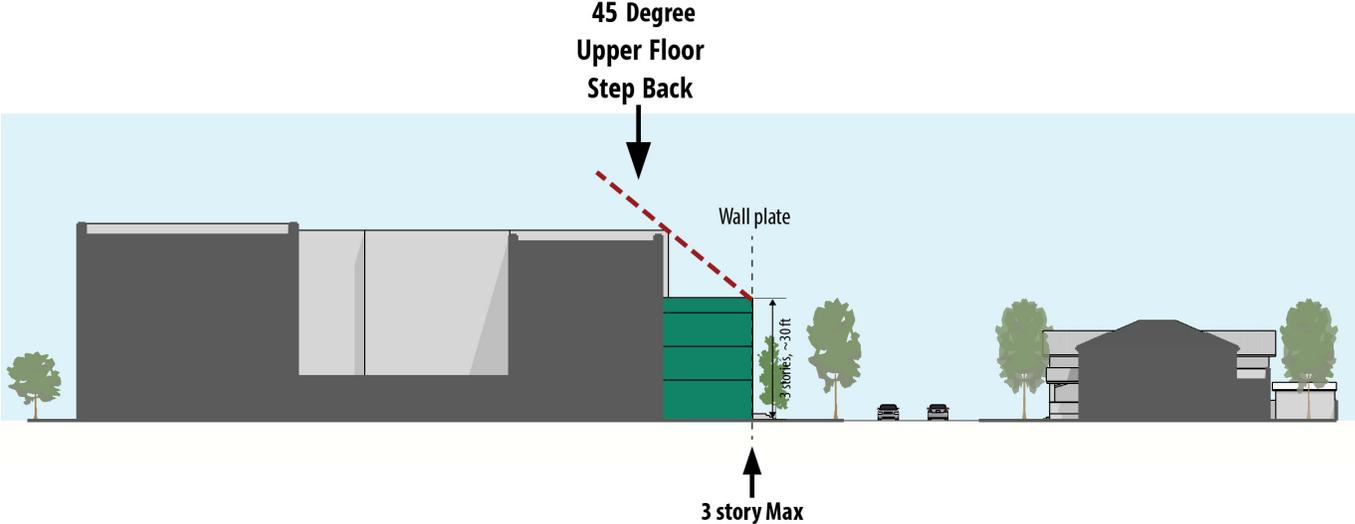
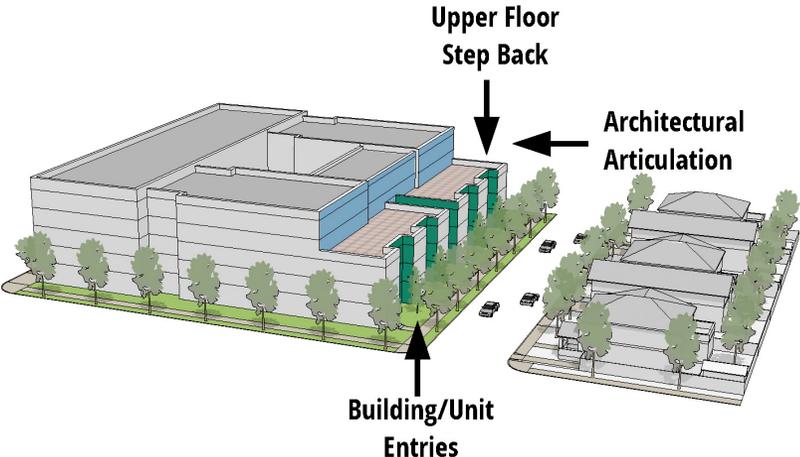
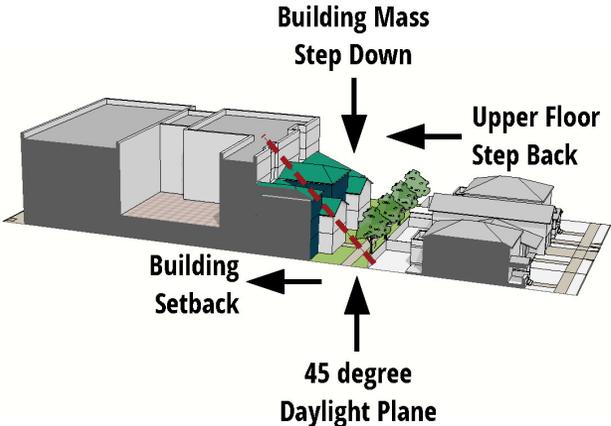
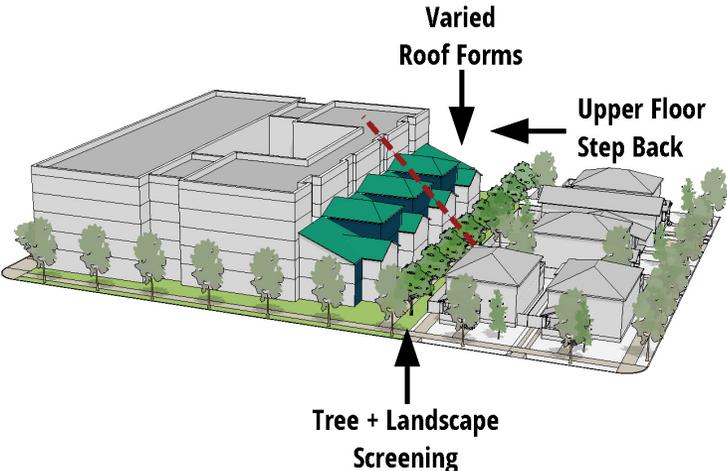


# Neighborhood Transition Strategies

- 45-Degree Daylight Plane
- Increased building setbacks
- Upper-story step backs
- Architectural articulation
- Smaller scale/house-form building types and varied roof lines
- Orientating primary windows and balconies away from existing homes
- Landscape buffers



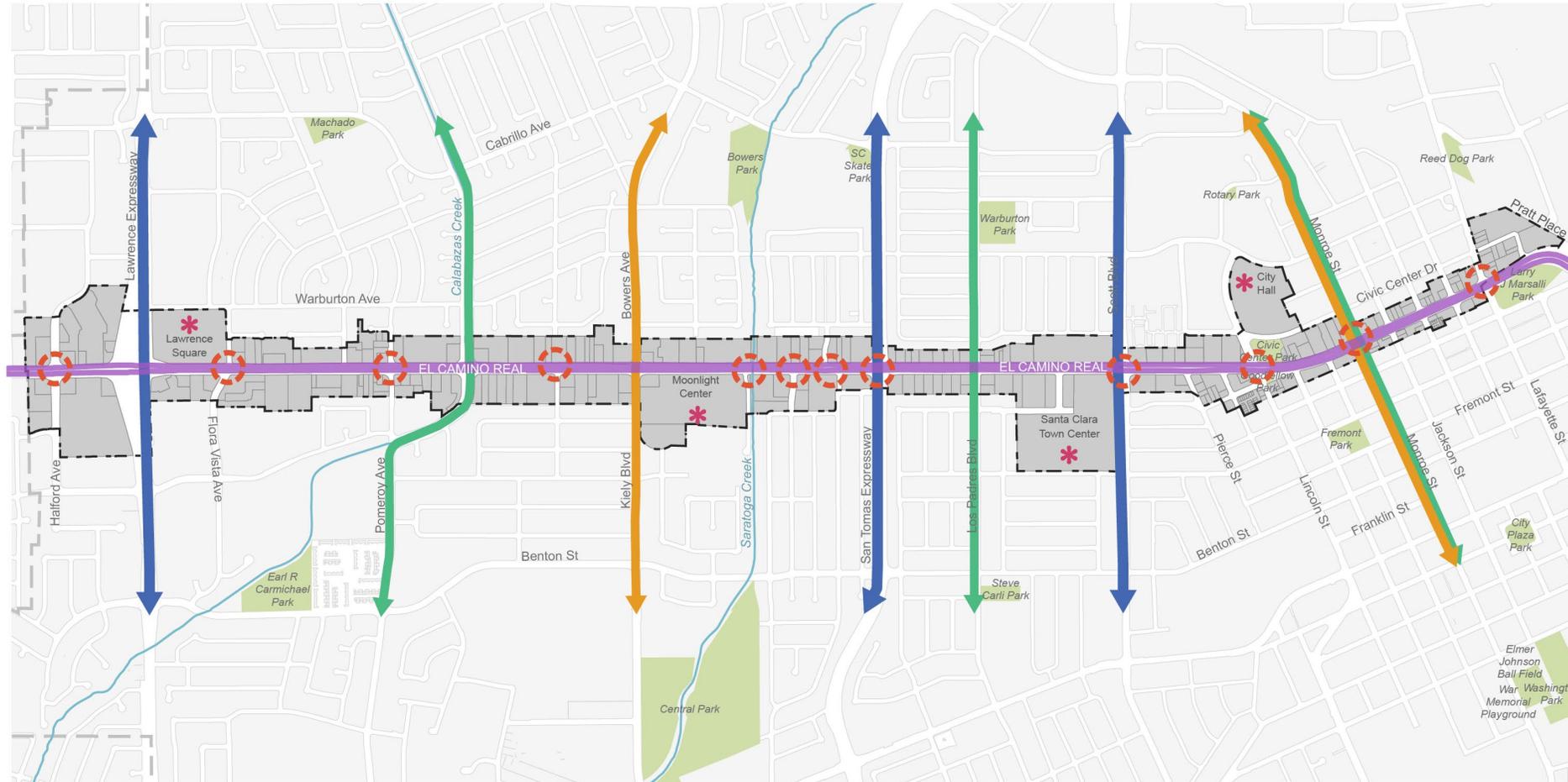
# Neighborhood Transition Strategies



# mobility framework & ROW concept

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# Mobility Framework

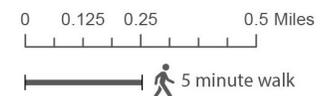


## Legend

- City Boundary
- Plan Boundary
- Parcels
- Plan Parcels
- Parks
- Creeks
- Civic Center
- Landmarks

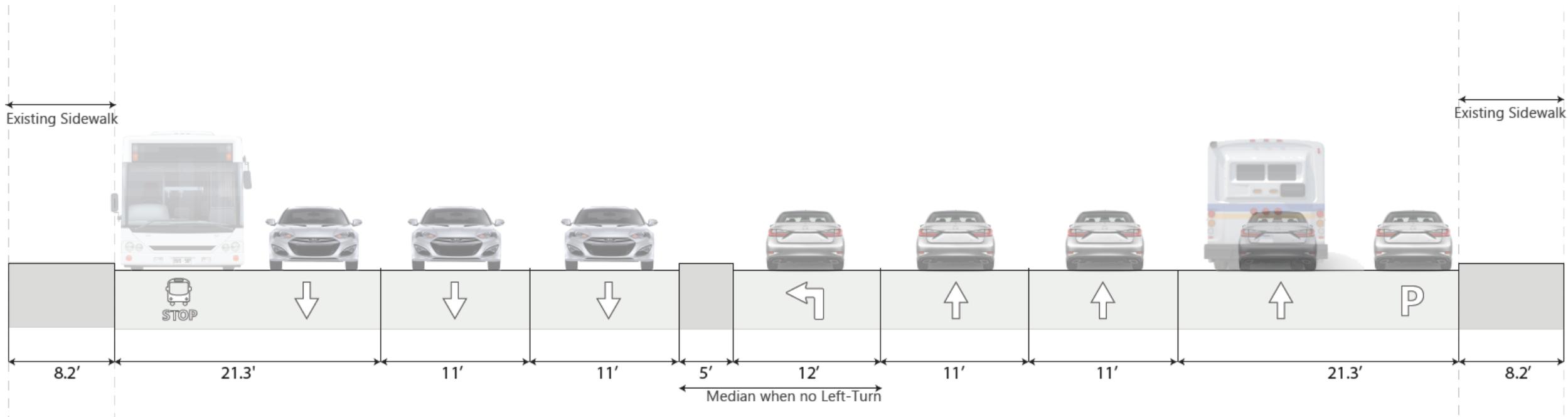
## Modal Priority

- El Camino Real - Multimodal Complete Street
- Vehicle Priority Access Corridor
- Bicycle Priority Access Corridor
- Transit Priority Access Corridor
- Priority Pedestrian Crossing Improvement



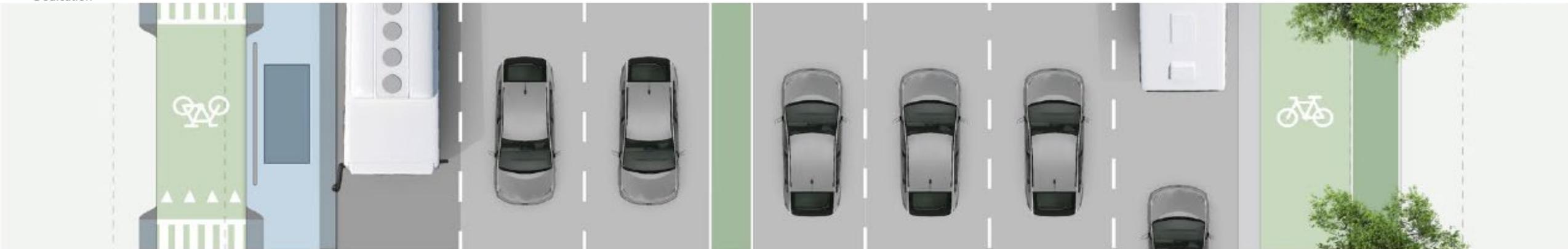
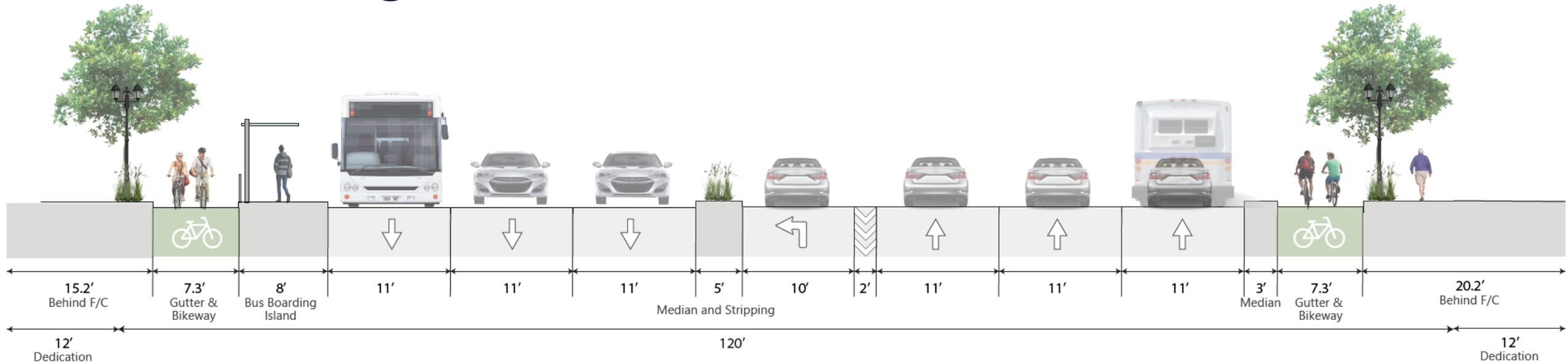
# ROW – Existing Conditions

- 3 travel lanes with left-turn lane, on-street parking and bus boarding. Narrow sidewalks; no bike facilities.



# ROW Concept

- Remove on-street parking to accommodate a cycle track and bus boarding islands; widen sidewalks and add street trees





**bus boarding island**

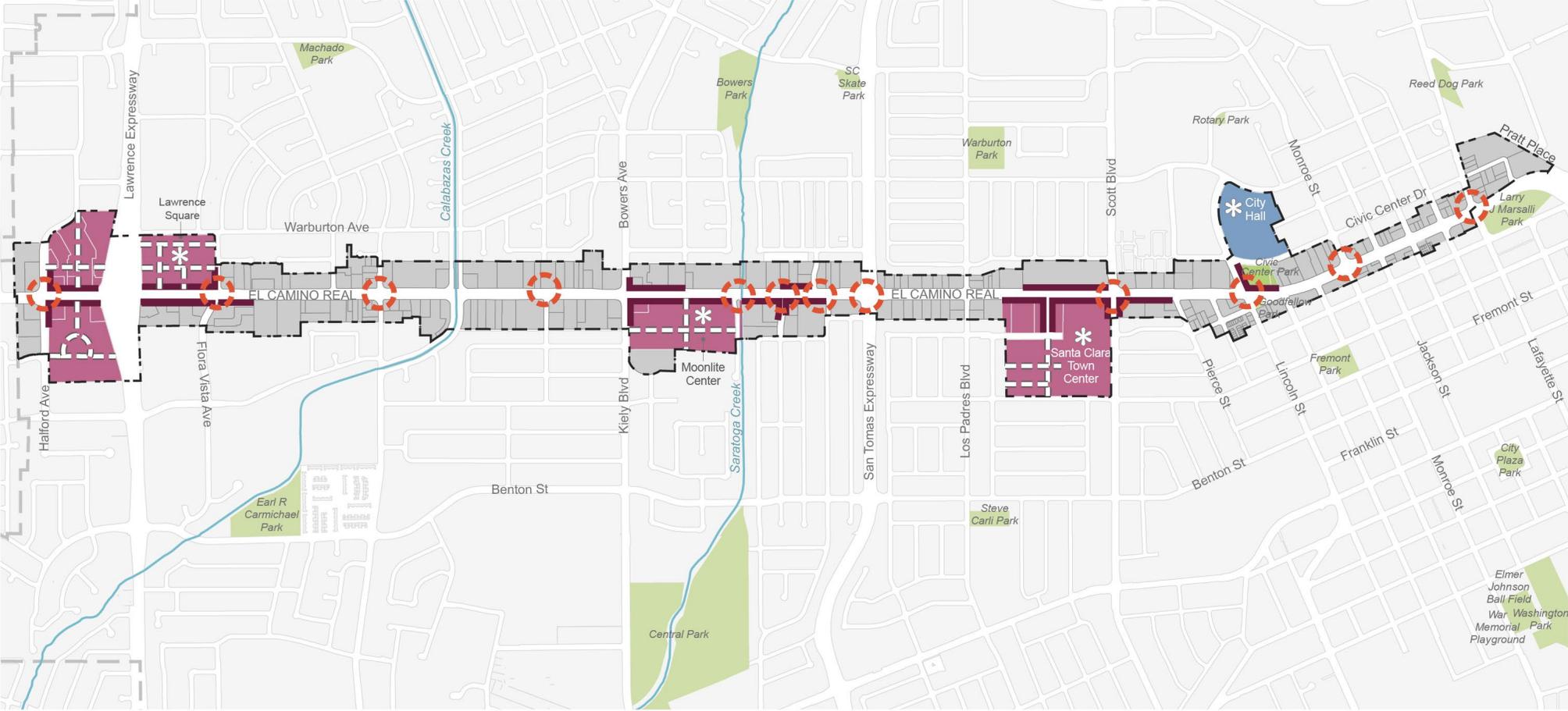


**cycle track**

# street & public realm improvements

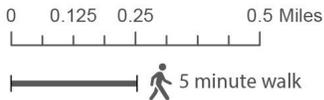
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# Priority Pedestrian Improvements



**Legend**

- City Boundary
- Plan Boundary
- Parcels
- ✳ Landmarks
- Parks
- Creeks
- Commercial Activity Center
- Civic Activity Center
- Priority Pedestrian Areas
- Priority Pedestrian Crossing Improvement
- New Street or Bike/Ped Connection (conceptual)





**Bulb out**



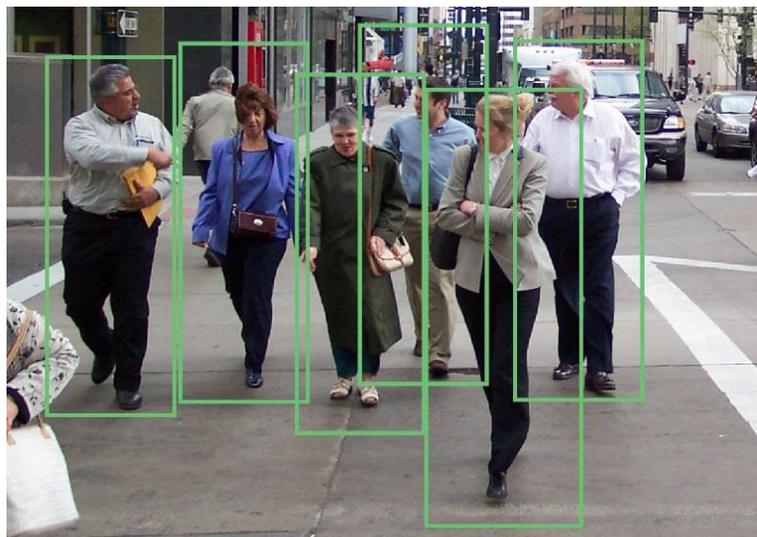
**Median Refuge Island**



**Decorative/ High-Visibility Crosswalk**



**HAWK Signal**



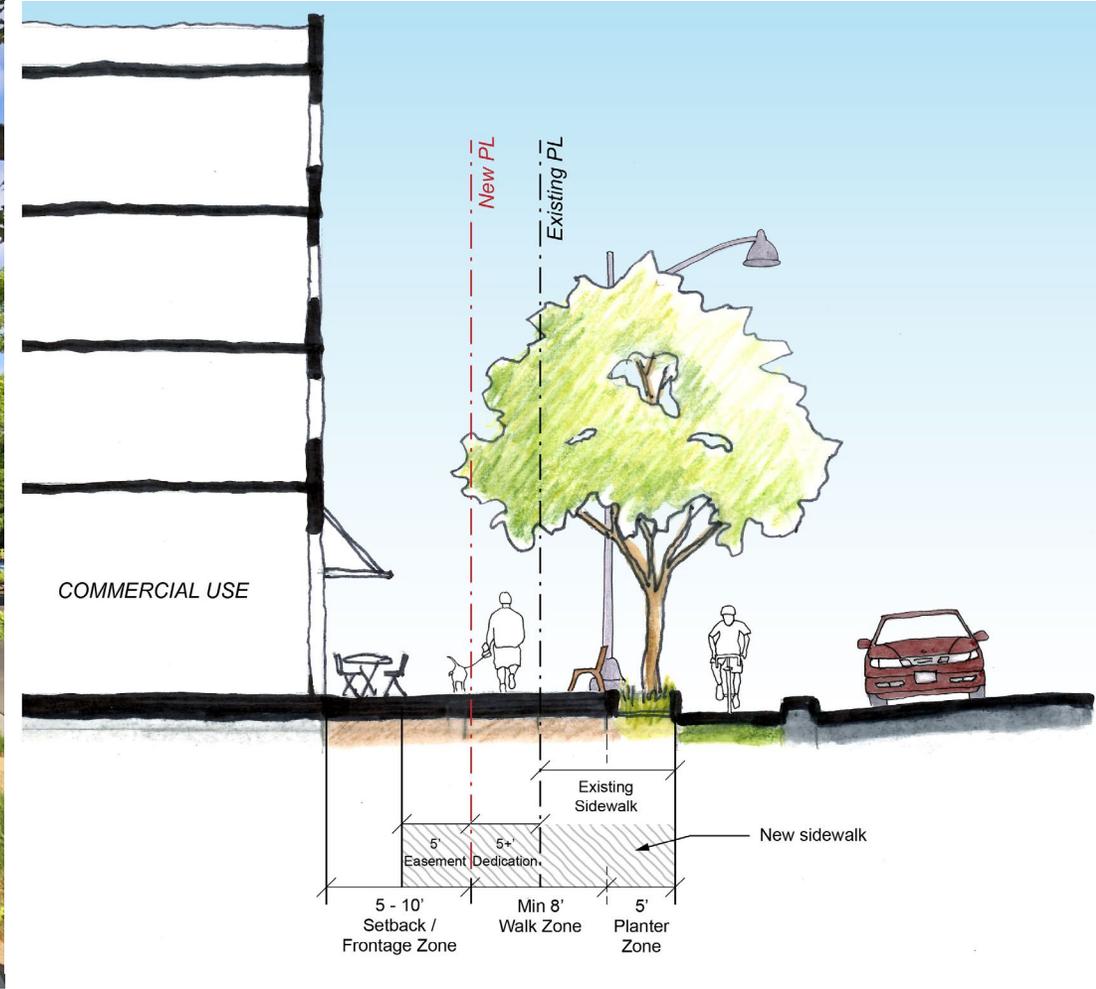
**Pedestrian Detection**

**Signal & Crossing Treatments**

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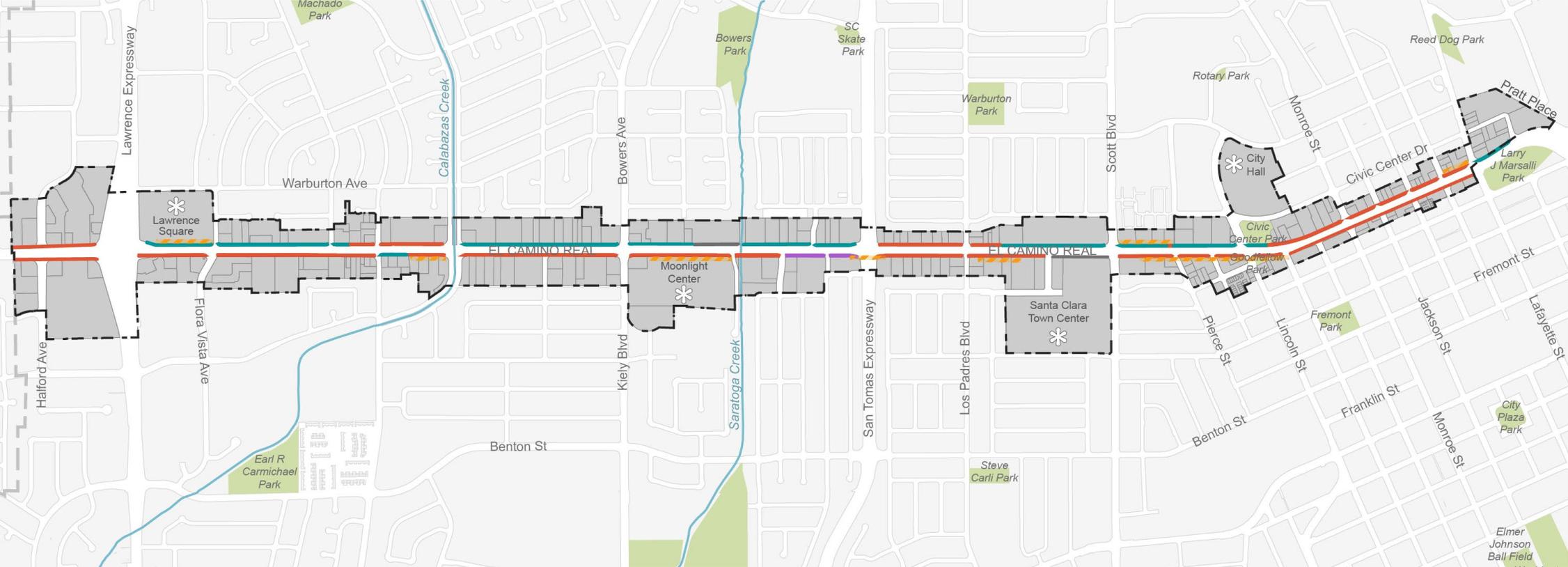
**Existing conditions**



# Proposed Pedestrian Improvements

20 ft sidewalks, pedestrian-scale furnishings, planter strip & street trees

# Utility Constraints

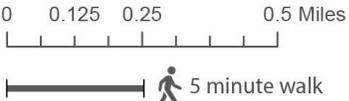


### Legend

- City Boundary
- Plan Boundary
- Parcels
- Plan Parcels
- Parks
- Creeks
- Landmarks

### Utility Conditions

- Condition A:** Potential for trees/rain gardens from face of curb 3 ft back
- Condition A - Existing:** Planting strip with street trees recently installed
- Condition B:** Storm drain and sanitary sewer lines would need to be relocated in these locations for street trees and landscaping to be planted in planter zone from face of curb approximately 3 ft back
- Condition C:** Multiple utility conflicts - street trees/rain gardens not possible
- Potential locations for permeable paving

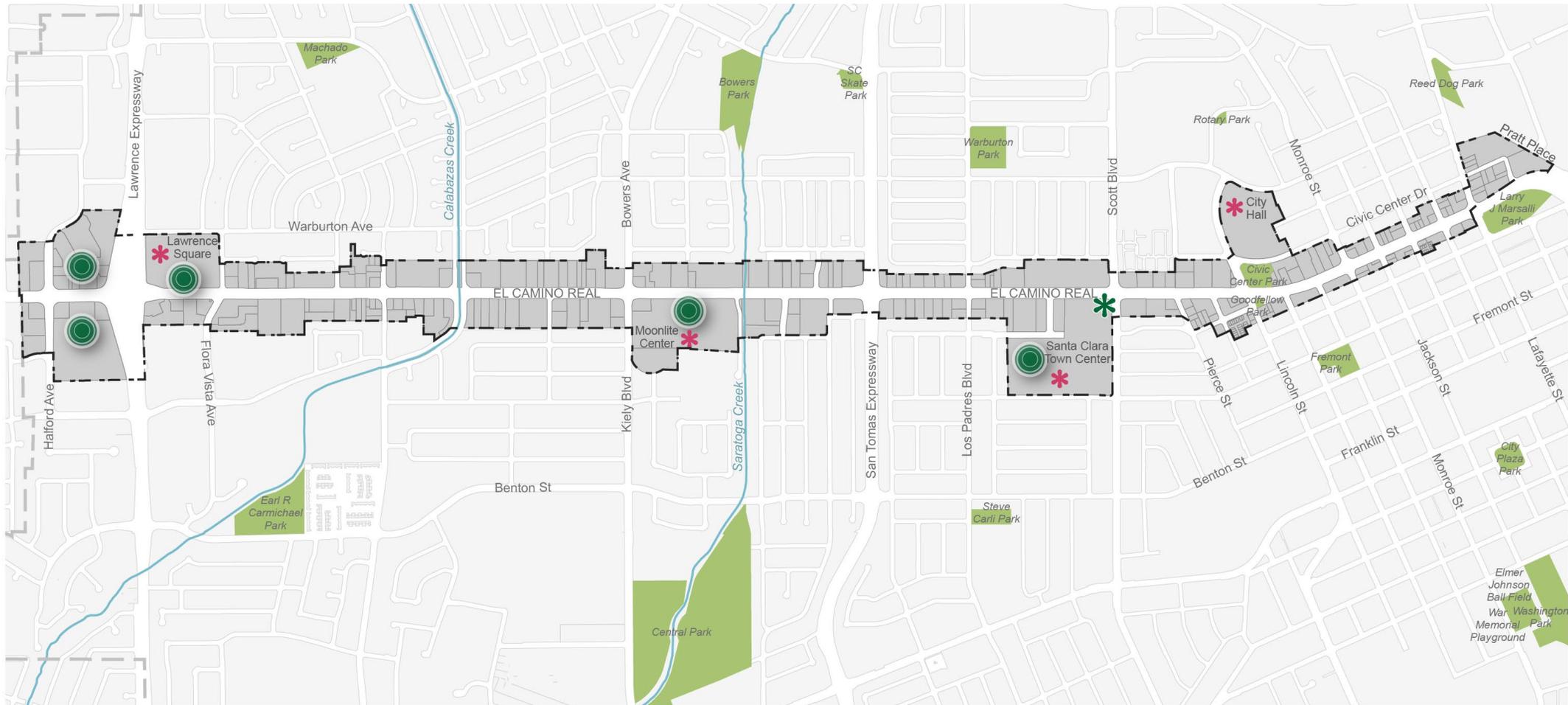


# Street Trees, Landscaping, & Stormwater

- Planter strips with landscaping & street trees
- Above-ground planters in areas with utility conflicts
- Curb cuts and stormwater flow-through planters
- Landscaped medians/buffers
- Permeable paving
- Native/drought-tolerant landscaping

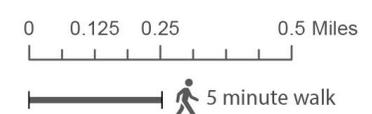


# Open Space Framework



## Legend

- City Boundary
- Plan Boundary
- Parcels
- ✱ Landmarks
- ✱ Existing Publicly Accessible Private Open Space
- Parks
- Conceptual Parks and Plazas
- Creeks





**publicly-accessible open space**

# Final Draft

- Edits from comments received on the Public Draft (*posted in May 2020*)
- Clarification on density calculation
- ROW diagrams – minor modifications to existing ECR ROW to reflect recent restriping; addition of an interim ROW diagram before bus boarding islands are constructed
- Add nearby schools to Bikeway and Modal Priorities maps; add Saratoga Creek as a bike priority corridor
- Add section on development/growth potential

# CAC Discussion / Q&A



# Public Comment



# Next Steps

- OPEN HOUSE – *virtual* public workshop
  - **Sept. 30, 5:30-7:00pm**
- PC/CC Sessions – Fall
- PC/CC Adoption Hearings – Late Fall/Winter

**Go to the website for more information and to stay involved!**

**[www.santaclearaca.gov/lecr](http://www.santaclearaca.gov/lecr)**

Project Contact:

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408.615.2484